



Planning Assessment Report

Trade supplies and Restricted retail premises

145 Glenlyon Road and 6 Pitt Street, Brunswick

July 2020

Prepared for**Brunswick Investment Project Pty Ltd****Project No
0925****Prepared by
Metropol Planning Solutions Pty Ltd****COLLINGWOOD**8 Gold Street
Collingwood VIC 3066PO Box 6081
Collingwood North VIC 3066**CAMBERWELL**Second floor
555 Riversdale Road
Camberwell VIC 3124t: 9882 3900
f: 9882 9969
e: info@metropolplanning.com.au
www.metropolplanning.com.au**Revision History**

Revision	Date	Details	Author / Position	Authorised	
				Name / Position	Signature
A	15 May 2020	Final Report	Shumin Hsieh Graduate Urban Planning Consultant	Michael Dunn Director	Original Signed
B	23 July 2020	Revised Report – RFI Response	Bethany Rote Planning Consultant	Michael Dunn Director	Original Signed

Contents

1.0	INTRODUCTION	4
2.0	PROPOSAL	7
3.0	SUBJECT SITE	8
3.1	Planning History	11
4.0	KEY ISSUES	12
4.1	Car Parking and Traffic	12
4.2	Off-Site Amenity Impacts	12
4.3	Consistency with Strategic Context	13
5.0	PLANNING POLICY	15
5.1	Planning Policy Framework	15
5.1.1	Clause 21 - Municipal Strategic Statement	17
5.1.2	Clause 22 – Local Planning Policies	20
6.0	PLANNING CONTROLS	23
6.1	Permit Triggers	23
6.2	Zoning	23
6.3	Overlays	25
6.4	Particular Provisions	29
6.4.1	Clause 52.02 – Easements, Restrictions and Reserves	29
6.4.2	Clause 52.05 – Signs	29
6.4.3	Clause 52.06 - Car Parking	30
6.4.4	Clause 52.34 – Bicycle Facilities	31
6.4.5	Clause 53.18 – Stormwater Management in Urban Development	32
7.0	AMENDMENT C193	33
8.0	CONCLUSION	34
	Appendix A – Site Photographs	35
	Appendix B – Property Interfaces Review	40

1.0 Introduction

This report has been prepared by Metropal Planning Solutions to support a planning permit application for the use and development of land for Trade supplies and Restricted retail premises (Bunnings Warehouse) at 145 Glenlyon Road, Brunswick. An easement is also proposed to be applied along the eastern boundary of 6 Pitt Street for access purposes. Together these properties form the subject site. This report has been updated as part of the preparation of our response to Council's request for further information dated 10 June 2020.

The design response has been informed by the preferred future character of the area as set out in the Brunswick Structure Plan and the Design and Development Overlay (Schedule 19) that applies to the site as set out in the Moreland Planning Scheme.

The site is currently occupied by a series of large former industrial buildings which are proposed to be demolished and replaced by a double storey Trade supplies and Restricted retail premises with ancillary in-store Café. The proposal represents an efficient use of land in a well-serviced area and is consistent with the aspirations of Council for land use and development within a well-established higher order activity centre, in close proximity to public transport and services.

The subject site is located within the Brunswick Major Activity Centre and the proposal will contribute significantly to local economic activity by significantly increasing the level of employment on the site. The proposed use will generate 120 jobs including full-time, part-time and casual. The site is currently underutilised, being a former factory that is now used for administration and warehousing activity and employs in the order of 30 full time, part time and casual jobs. The proposal is therefore entirely consistent with Council's vision for land within an activity centre where employment generating uses are strongly encouraged.

The subject site is surrounded by several residential buildings (to the north, west and east). Careful consideration has been given to these numerous residential interfaces to ensure the ongoing amenity of neighbouring properties. For example, a light court is incorporated into the design adjacent to a light court at 191-193 Lygon Street to provide for daylight access to neighbouring residents. The architectural plans demonstrate the considerable attention that has been paid to the various residential interfaces, including through detailed drawings of the light court and three-dimensional analysis of shadow impacts on neighbouring buildings and properties. The plans demonstrate that the impacts of the proposal on neighbouring properties are not unreasonable, particularly given the industrial zoning of the subject site and higher order activity centre location. Schedule 19 to the Design and Development Overlay provides considerable guidance as to how new buildings should be massed and upper levels setback to residential interfaces in the Brunswick Major Activity Centre and the proposal demonstrates consistency with these benchmarks.

Access (for cars, trade vehicles and heavy vehicles for deliveries and waste collection) and car parking has been thoroughly addressed in the architectural plans and traffic engineering assessment. There will be no unreasonable impacts on the operation of Glenlyon Road or Pitt Street from the proposal and sufficient car parking is provided on-site, noting that the majority of customers for a Trade supplies and Restricted retail premises are unlikely to use active modes of transport or public transport given the large, heavy and bulky items commonly purchased from such a business.

The proposal responds appropriately to the preferred future character of the area and is consistent with the outcomes sought for the site as set out in the State and local planning policies of the Moreland Planning Scheme as comprehensively detailed in this report and we therefore submit that the proposal is worthy of planning approval.

Our application comprises the following architectural plans prepared by Stokes Architects:

Drawing No.	Title	Comments
R00	Cover Sheet & Drawing Schedule	Cover page comprising a drawing list and 3D modelling image of the site.
R01	Existing Conditions	Aerial photograph of the site and information regarding surrounding buildings.
R02	Proposed Context Analysis	Proposal shown in relation to the surrounding buildings.
TP00	Survey	Plan of the survey result.
TP01	Site Plan	Images of the site and surrounds and site feature survey.
TP02	Basement 01 & 02 Plan	Demolition plan of the site.
TP03	Proposed Ground Floor Plan	Plan of the proposed development in the context of the immediately surrounding buildings.
TP04	Proposed Level 01 Plan	3D Render of the proposed development from Waterloo Road.
TP05	Proposed Roof Plan	Existing and proposed streetscape renders.
TP10	South Elevation	Enlarged existing and proposed streetscape renders.
TP11	West Elevation	Proposed West Elevation
TP12	North Elevations	Proposed North Elevation
TP13	East Elevations	Proposed East Elevations
TP15	Sections	Two north-south sections of the western portions of the proposed development.
TP16	Sections	Two east-west sections of the northern and western portions of the proposed development.
TP17	Sections	One north-south section of the eastern portion of the proposed development.
TP18	Neighbour Sections	Existing and proposed neighbour sections and plans.
TP20	Shadow Diagram 9AM on 21 September	Proposed shadow diagrams of the development at 9am on 21 September.
TP21	Shadow Diagram 10AM on 21 September	Proposed shadow diagrams of the development at 10am on 21 September.
TP22	Shadow Diagram 11AM on 21 September	Proposed shadow diagrams of the development at 11am on 21 September.

Drawing No.	Title	Comments
TP23	Shadow Diagram 12PM on 21 September	Proposed shadow diagrams of the development at 12pm on 21 September.
TP24	Shadow Diagram 1PM on 21 September	Proposed shadow diagrams of the development at 1pm on 21 September.
TP25	Shadow Diagram 2PM on 21 September	Proposed shadow diagrams of the development at 2pm on 21 September.
TP26	Shadow Diagram 3PM on 21 September	Proposed shadow diagrams of the development at 3pm on 21 September.
TP27	Perspectives	South-east and south-west street view along Glenlyon Road.
TP28	Perspectives	North-east and north-west aerial view proposed.
TP29	Perspectives	Proposed Pitt Street entry view.

This report and the architectural plans should also be read in conjunction with the following reports and plans:

- Traffic Engineering Report – prepared by TTM Consultants Pty Ltd
- Green Travel Plan – prepared by Sustainable Development Consultants
- Sustainability Management Plan – prepared by Sustainable Development Consultants
- Waste Management Plan – prepared by Sustainable Development Consultants
- BESS Assessment – prepared by Sustainable Development Consultants
- Acoustic Report – prepared by Octave Acoustics
- Development Impact Assessment (arboricultural) – prepared by Arbor Survey
- Boundary Re-establishment Feature and Level Survey (and associated photographic survey) – prepared by Veris
- Plan of Creation of Easement – prepared by Veris
- Environmental Site Assessment – prepared by ESG Environmental
- Acoustic Peer Review – prepared by Cogent Consultants

2.0 Proposal

The proposed development comprises a two-level commercial building constructed to the front boundary and extending across the site. An in-store café is also included within the building. Two levels of basement car park are proposed to accommodate staff and customers' cars. A total of 250 parking spaces including four accessible spaces will be provided.

The Glenlyon Road façade features a 14.2-metre-high street wall. This street wall height increases gradually to a parapet height of 15.4 metres setback 7.3 metres from the street frontage. The building is proposed to be constructed to the common boundaries with seven metres setback to the upper level to the north and west boundaries, save for the open nursery area at the north western corner of the site.

The building will be clad with metal panel cladding with expressed joints in a Bunnings green colour and sections of precast concrete wall panels. The proposed building integrates in terms of scale with the surrounding three and four storey residential / mixed use buildings.

The main retail area (titled main warehouse on the architectural plans) will occupy both ground and first floor of the proposed building at the southern end of the site and comprises a total area of 4,460m². An in-store café is located on the ground floor on south eastern corner of the main entrance with an approximate area of 25m². A trade sales area will occupy the northern section of the ground floor with an area of 1855m². An outdoor nursery is located on the first floor to the north west corner of the building and will consist of an outdoor terrace with a total area of 781m². A bagged goods section is located to the north east side of the first floor with an area of 1113m². An administration office will occupy a mezzanine level within the first floor with a total area of 225m².

Pedestrian access to the proposed building will be via a central location on the Glenlyon Road frontage; vehicle entry for customers (including trade business) is via a new crossover located at the south western corner of the site, where an existing crossover is located.

Heavy vehicle movements for deliveries and waste collection will access the site via a new crossover constructed at the south eastern corner of the site onto Glenlyon Road (this being an entry only access point), with egress via an accessway (carriageway easement) across land known as 6 Pitt Street to the north of the subject site.

A 2.84-metre-wide easement is proposed to be applied along the eastern boundary of 6 Pitt Street (formally identified as Lot 1 on Title Plan 53324M) for the purpose of allowing trade customers and heavy vehicles associated with deliveries and waste disposal to exit the subject site onto Pitt Street. This will provide a 6.5-metre-wide accessway width to allow for heavy vehicles to exit safely from the subject site. This adjoining property is under the same ownership as 145 Glenlyon Road and an agreement is in place with the owner to enable this easement to be created.

The two-level basement car park (for all staff and retail non-trade customers) will be accessed from the south-western corner from Glenlyon Road with egress from this same crossover. The vehicle accessway for use by heavy vehicles is located along the eastern boundary which allows goods to be unloaded in a dedicated loading bay. There is sufficient space for two trucks to queue off-street within the accessway if necessary, to wait for the loading bay to become available.

The operating hours of the business are 6am to 10pm Monday to Friday. Deliveries will be limited to between the hours of 7am and 6pm Monday to Friday and 7am and 1pm on Saturdays. Waste pick up will be restricted to between the hours of 7am and 6pm Monday to Friday and 7am and 1pm on Saturdays.

Business identification signage comprising the standard Bunnings text and logo will be located on the western, southern and northern façades of the proposed building. The northern façade features a non-illuminated sign totalling 35 square metres. The proposed sign to the southern façade (along Glenlyon Road) is proposed to be externally illuminated and occupies an area of 108 square metres on the front façade of the building.

Two signs are proposed to the western façade. The smaller sign to the south (55 square metres) is proposed to be externally illuminated while the larger sign closest to the north (and closer to neighbouring residential uses) has a total area of 141 square metres and is not proposed to be illuminated. Externally illuminated (floodlit) signs will be illuminated via outriggers extending out from the façade and will direct all light back downwards onto the façade. The external illumination will cease at 11pm.

3.0 Subject Site

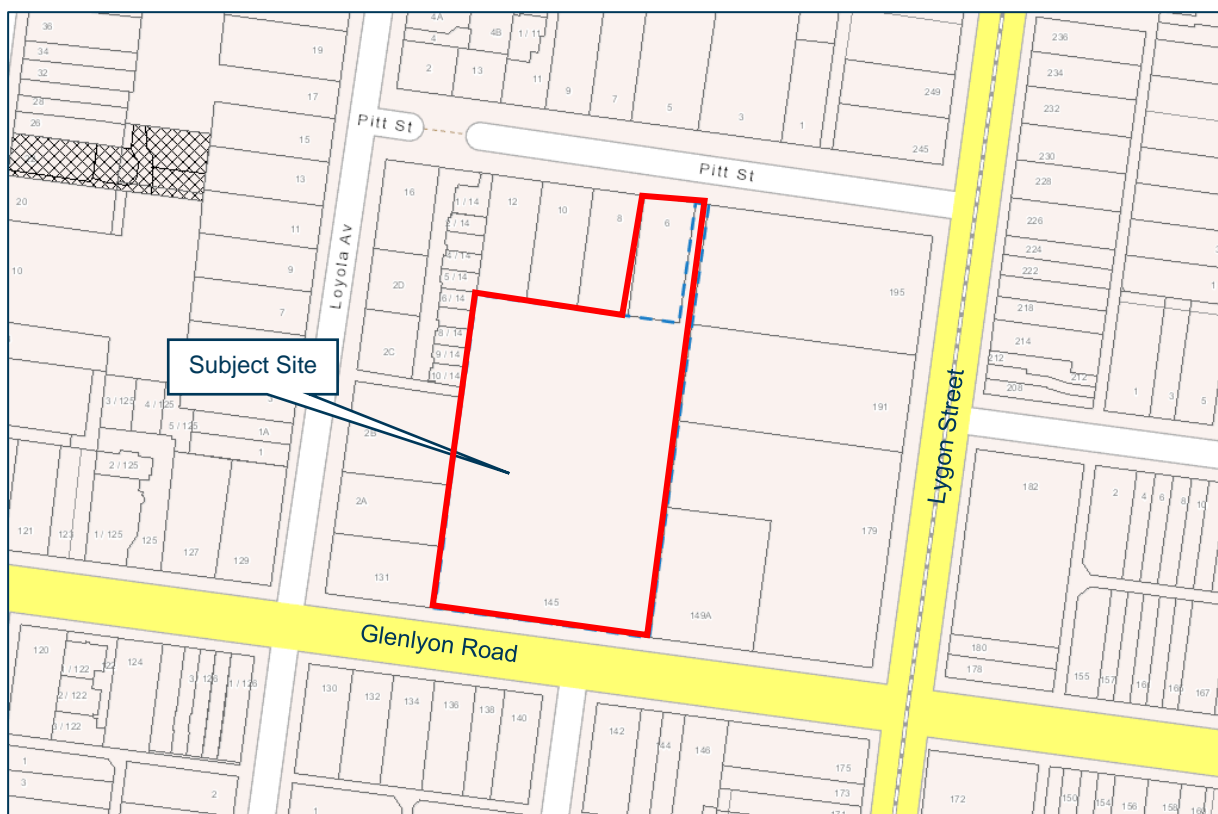
The subject site consists of two lots with the address of 145 Glenlyon Road and 6 Pitt Street Brunswick.

The property known as 145 Glenlyon Road has a parcel description of Lot 1 on Title Plan 443505J and a site area of 5395m². The application also applies to the easternmost portion of 6 Pitt Street with a parcel description of Lot 1 on Title Plan 53324M, for which a carriageway easement is proposed to be applied along its eastern boundary. Refer Figure 1.

The site known as 145 Glenlyon Road is occupied by a two-storey commercial building constructed to the front boundary, with a series of warehouse/ former factory buildings to the rear. The warehouse/ former factory buildings are constructed to the eastern and northern and a short section of the western boundaries (at the north-western corner of the site). An open handstand area for car parking is located centrally on the site. The property known as 6 Pitt Street is currently occupied by a car park associated with a commercial kitchen located at 8 Pitt Street.

A recent search of the Certificate of Title (searched 16 April 2020) has confirmed that the subject site is not affected by any restrictions on title.

Figure 1: Cadastral Plan



Source: <https://mapshare.vic.gov.au/vicplan/>

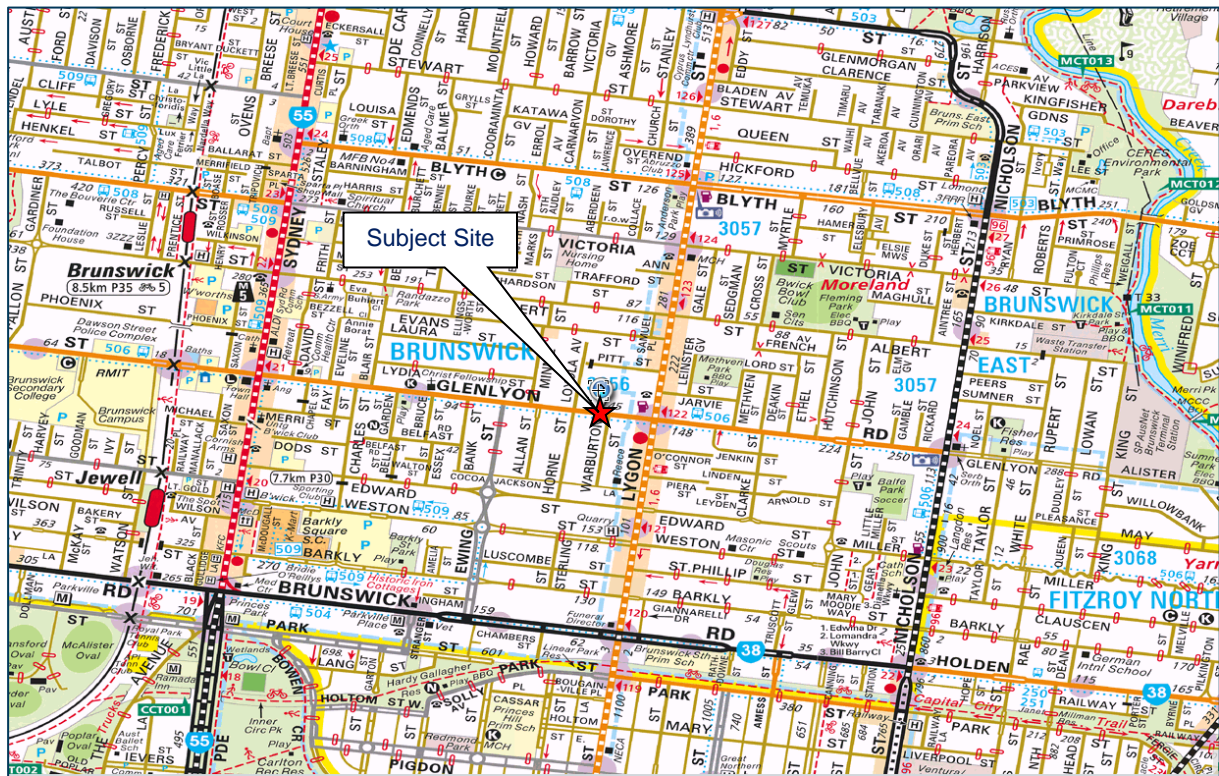
The subject site is within the locality of Brunswick within the municipality of the City of Moreland. Refer Figure 2.

The site is located within Brunswick Major Activity Centre. The subject site is located in close proximity to numerous services and shops. The Barkly Square Shopping Centre is located to the south west of the site which consists of various retail stores and restaurants. The subject site is located at the periphery of the residential neighbourhood of Brunswick.

The subject site enjoys excellent access to public transport reflective of its location within the Brunswick Major Activity Centre. The site is in close proximity to Tram Route 1 and 6 located along Lygon Street and is also located along the route of Bus 506 route which connects Moonee Ponds and Northcote. Trams and buses provide access to surrounding suburbs and Melbourne Central Business District (CBD).

The subject site is located on the northern side of Glenlyon Road and has excellent access to the surrounding road network. Glenlyon Road runs east-west direction and acts as a collector road to provide access to the surrounding suburbs such as Moonee Ponds and Northcote.

Figure 2: Site Locality Plan



Source: <http://www.street-directory.com.au/>

Figure 3 and Figure 4 provide aerial photographs of the site and its surrounds at different scales and provide an indication of the existing conditions, including buildings, roads and vegetation.

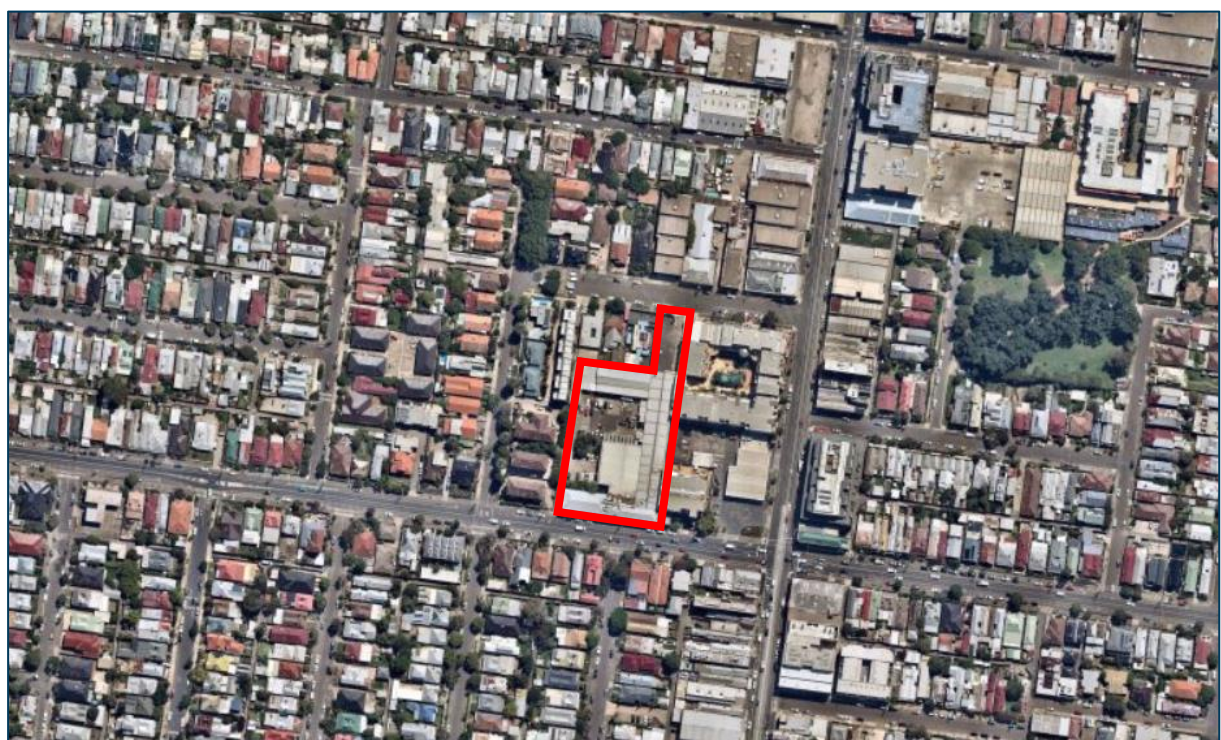
Figure 3 illustrates the site and its immediate surrounds. Land to the north, west and south of subject site is used predominantly for residential uses whilst the area to the immediate east of the subject is used for commercial and residential uses.

Figure 3: Aerial photograph of site and immediate surrounds



Source: Nearmap – Image Date 17 February 2020

Figure 4: Aerial photograph of the subject site and broader surrounds



Source: Nearmap – Image Date 17 February 2020

Figure 4 shows the site within its broader surrounds. Most land to the south and west is used for residential purposes and land to the east is used predominantly for commercial / mixed use purposes. A comprehensive assessment of the immediate interfaces of the subject site is included in Appendix B.

3.1 Planning History

Planning permit MPS/2018/1010 applies to 6-8 Pitt Street for buildings and works associated with a take away food premises.

We understand that the proposed creation of the easement at 6 Pitt Street would be in conflict with the existing use of 8 Pitt Street for a take away food premises with car parking associated with this use provided on 6 Pitt Street. It is intended that the permit for the take away food premises would be amended to resolve this issue, or the use will cease upon the commencement of the use of the land for Trade supplies and Restricted retail. We also note that the subject site and 6-8 Pitt Street are under the same ownership.

4.0 Key Issues

4.1 Car Parking and Traffic

The proposal comprises a two storey Trade supplies and Restricted retail premises (Bunnings Warehouse) that includes two basement levels for car parking. The car parking area will provide a total of 250 car spaces (including four accessible parking spaces) for staff and customers.

The proposed basement car park is designed to be accessed from Glenlyon Road via a crossover located at the southwestern corner of the site where the existing crossover is located.

A heavy vehicle accessway is proposed to be located along the eastern boundary for goods to be loaded and unloaded at the dedicated loading bay. There is sufficient space for two trucks to queue off-street within the accessway if necessary to wait for the loading bay to become available. The accessway can be entered from the south-eastern corner from Glenlyon Road and exited from the north-eastern corner onto Pitt Street.

Response

The comprehensive Traffic Impact Assessment prepared by TTM Consulting which forms part of our application package finds that the proposed use will not have any adverse impact on the local road network and that the movement of cars, trade vehicles, delivery vehicles and waste vehicles within the site will function safely and efficiently.

The provision of 250 car parking spaces is sufficient to satisfy peak demand generated by the proposed use.

The subject site enjoys excellent public transport connections. The subject site has direct access to the Melbourne CBD and to surrounding suburbs via Tram route 1 and 6 which operates along Lygon Street approximately 95 metres to the east of the site. Additionally, Bus route 506 that operates along Glenlyon Road provides excellent public transport access to Moonee Ponds and Westgarth.

While active and public transport modes may not be practical for most customers given the nature of the use, it is considered that a portion of staff and customers will use public transport to access the site, given the site's inner-city location and within a highly pedestrianised part of Brunswick. Additionally, bicycle parking has been provided to support sustainable transport objectives.

We submit that vehicle access via Glenlyon Road in the proposed location is appropriate and will not result in significant impact upon the safe and efficient function of this road. The use of the Pitt Street exit for delivery and waste vehicles and trade traffic is appropriate and will not impact upon the function of this local street.

4.2 Off-Site Amenity Impacts

Sensitive residential interfaces adjoin the subject site to the north-west, west, north and east.

The proposal has the potential to result in amenity impacts on adjoining residential properties due to noise, visual bulk, overshadowing and loss of access to daylight.

Response

The design response achieves an appropriate balance between ensuring reasonable ongoing amenity of residents of adjoining properties while achieving a development outcome which makes efficient use of a large industrial zoned site located within an area marked for significant change (which is within a higher order activity centre).

The degree of sensitivity of each of the adjoining properties has been thoroughly assessed and documented in the interface study provided in Appendix B of this report. The design response has been informed by this interface study including through the incorporation of a light court opposite an existing light court on the adjoining property to the east of the subject site at 191-193 Lygon Street, to ensure the ongoing amenity of residents of these dwellings.

The Moreland Planning Scheme provides considerable guidance (via DDO19) as to how development located at the interface between residential zoned land and the Brunswick Major Activity Centre should be treated in terms of building height and setback. The overlay sets out a preferred maximum height of 14 metres for the subject site and a setback of a dimension equivalent to its height above five metres, up to a maximum setback of ten metres from the residential boundary. Sectional Drawings C, D & F (Plans TP16 & TP17) show the proposed building with the DDO residential interface setback line. These sectional drawings demonstrate that while there are minor non-compliances on the boundary towards the rear of the site (Sections D & F), much of the building envelope beyond the boundary comfortably achieves compliance, including all of the building envelope at the more sensitive location towards the northern end of the site opposite the contemporary townhouses at 14 Pitt Street, thus ensuring visual bulk impacts on neighbouring residential properties are not unreasonable.

An acoustic report has been prepared by Octave Acoustics to assess the potential noise impact of the use on nearby residential properties. This assessment finds that the mechanical plant associated with the proposed development presents a low risk of exceedance of the relevant noise criteria (SEPP N-1) due to its anticipated rooftop location. Noise from the loading bay located to the eastern boundary of the subject site is expected to comply with SEPP N-1 during its hours of operation which will be restricted to between the hours of 7am and 6pm Monday to Friday and 7am and 1pm on Saturdays. A small timber cutting shop (panel saw and docking saw) will be located within the timber trade sales area on the ground floor level. The acoustic assessment found that noise from timber cutting will comply with SEPP N-1. The design response has appropriately addressed the numerous residential interfaces of the site and ensures the ongoing amenity of neighbouring properties.

The architectural plans provide a detailed shadow analysis of the proposal with respect to immediately abutting residential properties, including a three-dimensional analysis of adjoining properties. The shadow analysis demonstrates that while there will be shadow impacts on adjoining properties these will not be unreasonable having regard to the context of the site.

The proposal will result in a high building envelope in some locations. We submit that the visual bulk impacts of the proposal are not unreasonable given the context of the site within an industrial zone and given that the degree of sensitivity of many of the adjoining residential properties is not overly high, as detailed in the interface analysis provided in Appendix B.

4.3 Consistency with Strategic Context

The proposal consists of a two storey Trade supplies and Restricted retail premises (Bunnings Warehouse) and is located within the Industrial 3 Zone. The subject site is within the Brunswick Major Activity Centre and is located within the Lygon Street Central Precinct as identified in the Brunswick Structure Plan prepared in 2010.

The subject site is also classified as being within an Employment Areas (Category 2) within the Moreland Industrial Land Strategy (MILS). Within Category 2 area a mix of industrial uses and a transition to office and retail uses are encouraged, broadening the business base and employment opportunities available and complementing the diversity of land uses within Activity Centres.

Amendment C193 to the Moreland Planning Scheme proposes to rezone the Industrial 3 zoned land in this area (including the subject site) to Commercial 3 Zone to support the transition from traditional industrial uses to other employment uses and prioritise employment uses over residential uses.

Response

The design response is consistent with the strategies set out in the Brunswick Structure Plan 2010 (which is the basis for planning policy and controls in this area) and supports the objectives specified in the MILS for Employment Areas (Category 2).

The site is located within the Brunswick Major Activity Centre and is zoned Industrial 3. The purposes of the Industrial 3 Zone include to provide industries and associated uses in specific areas and allow limited retail opportunities in appropriate locations.

The subject site is within Lygon Street Central Precinct of the Brunswick Major Activity Centre where the land use objective is to ensure that new development provides a balanced mix of uses and supports local employment.

The MILS seeks to strengthen the economic role of the Brunswick Major Activity Centre to ensure a more competitive local economy that provides employment opportunities and supports the local community. The MILS also supports the transition from traditional industrial uses to a broader range of employment uses and seeks to prioritise employment uses over residential uses. The proposal will provide significant future employment opportunities with a net increase in employment on the subject site of 90 jobs when compared to the existing business that occupies the site. The proposal will provide significant economic activity and support to the local economy.

The proposal is consistent with the objectives of the Brunswick Structure Plan and the MILS and will contribute positively to local employment and economic activity.

5.0 Planning Policy

5.1 Planning Policy Framework

The Planning Policy Framework (PPF) sets out the State Planning Policies which apply to all land in Victoria. The PPF is structured around nine themes, the themes that are of most relevance to this proposal as discussed in this section of the report.

The PPF recognises the need for integrated decision making which recognises and responds to the needs and expectations of the community relating to land for settlement, protection of the environment, economic well-being, various social needs, proper management of resources and infrastructure. These needs are to be met through land use planning by addressing those aspects of economic, environmental and social wellbeing that are affected by land use and development. Conflicting objectives are to be balanced in favour of net community benefit and sustainable development for the benefit of present and future generations.

The PPF strongly encourages economic activity within designated areas as facilitated by land use planning. Economic development should meet the communities' needs and provide a net community benefit in relation to accessibility, efficient infrastructure use and the aggregation and sustainability of commercial facilities.

Clause 11.03-1S (Activity Centres) is set out to encourage the concentration of major retail, residential development into activity centres that are highly accessible to the community.

Relevant strategies include:

- *Build up activity centres as a focus for high-quality development, activity and living by developing a network of activity centres that:*
 - *Comprises a range of centres that differ in size and function.*
 - *Is a focus for business, shopping, working, leisure and community facilities.*
 - *Provides different types of housing, including forms of higher density housing. Is connected by transport.*
 - *Maximises choices in services, employment and social interaction.*
- *Support the role and function of each centre in the context of its classification, the policies for housing intensification, and development of the public transport network.*
- *Undertake strategic planning for the use and development of land in and around activity centres*
- *Support the continued growth and diversification of activity centres to give communities access to a wide range of goods and services, provide local employment and support local economies.*
- *Encourage economic activity and business synergies. Improve the social, economic and environmental performance and amenity of activity centres.*

Response

The proposal will contribute towards the efficient and sustainable use of infrastructure and services in an established urban area within a higher order activity centre. The land is fully serviced and has access to various modes of public transport all within walking distance.

The proposal will contribute to local economic activity. The development has been designed to achieve an urban design outcome in keeping with its physical and strategic context. The proposed Trade supplies and Restricted retail premises use will encourage the growth of the Brunswick Major Activity Centre and provide increased local employment opportunities to support the local economy.

The redevelopment of the site for Trade supplies and Restricted retail premises, replacing its former industrial and current warehouse uses reflects the transition of inner-city industrial areas to more intensive economic uses and encourages increased economic activity and greater local employment opportunities.

The Built Environment and Heritage theme is addressed in Clause 15 of the PPF. This Clause identifies that planning should ensure that new land use and development responds appropriately to its context and recognizes that quality built environments supports the social, cultural, economic and environmental wellbeing of communities. Planning should achieve high quality urban design and architecture that contributes to the local urban character and sense of place and reflects the characteristics and aspirations of the community. New development is to enhance livability, diversity and amenity and safety of the public realm and make places more attractive.

Development proposals are to include a site analysis and descriptive statement which explains how the proposal responds to the site and its context.

Response

The design response has been prepared by a well-respected local architectural practice and demonstrates a high-quality architectural response that is site responsive.

The proposed building is constructed to the Glenlyon Road frontage with a street wall height of 15.4 metres and will present to the street as a double-storey partially glazed façade, bringing an enhanced level of activation to the Glenlyon Road frontage when compared to existing conditions.

The proposed buildings and works are a sympathetic response to the surrounding character. The existing buildings do not contribute to local heritage and thus the proposed demolition will not result in any loss of local heritage values.

The Sustainability Management Plan prepared by Sustainable Development Consultants demonstrates that the proposal will achieve a high standard of energy efficiency for the future tenant (Bunnings).

The subject site is well located to encourage active and public transport modes for future staff and customers, rather than being dependent upon private motor vehicle use.

Clause 17 of the SPPF addresses the theme of Economic Development and aims to ensure planning contributes to the economic well-being of communities and the State as a whole. A key objective relevant to the proposal is to encourage development that meets communities' needs and provide a net community benefit. Strategies to achieve this aim include directing local commercial facilities into existing or planned activity centres.

Response

The proposed use is in keeping with the objectives of Clause 17 of the PPF. The subject site is located within an existing higher order (major) activity centre. The proposed Trade supplies and Restricted retail premises will provide goods that meets the needs of local residents and will contribute to local economic activity.

Clause 18 of the SPPF addresses the theme of Transport and aims to ensure that transportation and land use planning is integrated. Sustainable modes of personal transport, including walking and cycling are to be encouraged

by creating environments that are safe and attractive. Provision is to be made for car parking appropriate for the proposed uses.

Response

The subject site has excellent public transport connections and is located within the Principal Public Transport Network Area (PPTNA), by having direct access to Melbourne CBD and suburbs to the north and south via tram routes 1 and 6 that operate along Lygon Street, as well as good access to suburbs to the east and west such as Moonee Ponds and Northcote via bus services along Glenlyon Road.

5.1.1 Clause 21 - Municipal Strategic Statement

The Municipal Strategic Statement (MSS) of the Moreland Planning Scheme provides a statement of the key strategic planning, land use and development objectives for the municipality and the strategies and actions for achieving these objectives.

The City of Moreland is an established urban municipality in the inner north of Melbourne with a steadily growing population, and a high level of cultural and linguistic diversity. Clause 21.01 addresses key planning issues facing the City of Moreland, including the following:

- *Population growth and associated needs for housing, infrastructure, community facilities, employment and services*
- *Industry is in transition, reflecting the changing nature of manufacturing and growth in the service economy. The ratio of local jobs to residents in the workforce is low compared to the Melbourne average. The vast majority of Moreland residents in the workforce travel outside the municipality to work in nearby areas. There is a desire to provide greater opportunities for Moreland residents to work locally to reduce travel times to work, reduce congestion and for the sustainability and health benefits of less car reliance.*

Clause 21.02-3 of the MSS addresses strategic directions for activity centres, and Land for Industry and Economic Regeneration, more specifically for Employment Areas (Category 2) which includes the subject site.

The strategic direction for commercial development within this area is to generate employment opportunities and boost the local economy. The subject site is located within an Industrial 3 Zone but is also categorised within an Employment Area (Category 2).

The MSS include specific directions to support the transition from traditional industrial uses to a broader range of employment uses and prioritise employment uses over residential uses within Employment Areas.

Three Industrial Precincts (core industrial areas) have been identified for long term retention in zones that facilitate industry and employment uses and prohibit new residential uses. These three precincts are relatively unconstrained by residential or other sensitive uses and are intended to remain as priority areas for long term investment in industrial and other compatible businesses. The subject site is not located in one of these core industrial areas.

The subject site is located within the Brunswick Major Activity Centre and has been identified as within an Employment Area (Category 2). This reflects the strategic direction for this area which is to maintain industry and employment uses and transition to a broader business base which contributes to economic regeneration and more diverse employment opportunities.

Council is committed to best practice environmentally sustainable development (ESD). Development should integrate the principles of ESD early in the design process and at the planning stage.

Clause 21.03 provides the MSS objectives and strategies to implement the vision set out in Clause 21.02. The relevant sections of this clause relate to Activity Centres, Land for Industry and Economic Regeneration, Environmentally Sustainable Development and Transport Network.

Clause 21.03 of the Moreland Planning Scheme states that activity centres within the municipality are to provide a broad mix of retail uses to offer a diverse mix of employment opportunities. Brunswick Major Activity Centre is to accommodate significant change and intensification within the municipality.

Relevant objectives of Clause 21.03-1 include:

- *To support a network of activity centres across Moreland to provide residents with walkable access to their daily and weekly shopping and service needs.*

Strategies to achieve these objectives include:

- *Encourage the Coburg, Brunswick and Glenroy Activity Centres to provide a broad mix of retail uses, commercial and cultural activity, employment options, administrative and civic centre functions, government investment and regional facilities, in accordance with the relevant zones and overlays, and the Glenroy Structure Plan 2008.*
- *Encourage Neighbourhood Centres to serve the daily and weekly shopping and service needs of the local community.*

Clause 21.03-1 of the MSS addresses Land for Industry and Economic Regeneration issues within the municipality. Objectives of Clause 21.03-1 include:

- *To facilitate change to the scale of the built form within activity centres in accordance with their size and role in the activity centre network.*
- *To support activity centres as important commercial and employment clusters.*

Strategies to achieve these objectives include:

2.1 Encourage the Coburg, Brunswick and Glenroy Activity Centres to accommodate substantial growth and change.

Clause 21.03-2 of the MSS addresses Land for Industry and Economic Regeneration issues within the municipality. Objectives of Clause 21.03-2 include:

- *To support the transition from traditional industrial uses to a broader range of employment uses and prioritise employment uses over residential uses within Employment Areas (Category 2).*

Strategies to achieve these objectives include:

5.1 *Protect the continued operation of existing viable industries in Employment Areas.*

5.2 *Facilitate the transition of Employment Areas to accommodate a broader range of employment generating uses, including a mix of industry and office based uses and other compatible employment uses.*

5.4 *Encourage flexible floor plate layouts and increased floor to ceiling heights at ground and first floor to facilitate a variety of employment uses over time.*

5.6 *Encourage building design and site layouts of industrial and commercial uses in Employment Areas to minimise the potential for adverse amenity and environmental impacts on nearby residential uses.*

5.7 *Encourage redevelopment in Employment Areas to be of a high quality to contribute to an overall improvement in the amenity of the area, including low maintenance landscaping and upgrades to adjoining streetscapes*

5.8 *Encourage the amenity expectations for residential or other sensitive uses within or adjacent to Employment Areas to be reflective of the multi use nature of the areas and the priority given to employment uses.*

Proposed development should satisfy the objective by ensuring development responds and contributes to its context and any relevant heritage significance.

Relevant strategies include:

10.1 *ensure the overall siting and building envelope is based on a considered analysis of the site's context and features, including site size, shape and orientation.*

10.2 *ensure site design, building frontages, design articulation and internal layout achieve a good interface with and surveillance of the public realm.*

10.6 *ensure service infrastructure such as substations are well concealed or integrated into building design.*

10.9 *ensure signage is sensitive to the style, scale and character of host buildings, nearby buildings, and streetscapes.*

Clause 21.03-5 addresses environmentally sustainable development within the municipality. The objective of Clause 21.03-5 is:

- *To encourage development to contribute to environmental sustainability.*

A strategy to achieve this objective includes:

13.1 *Encourage development to incorporate environmentally sustainable design in the following areas: energy efficiency, water resources, indoor environment quality, stormwater management, transport, waste management and urban ecology in accordance with Clause 22.08 Environmentally Sustainable Development.*

Response

The Municipal Strategic Statement (MSS) of the Moreland Planning Scheme recognises the key planning issue of industries in transition resulting from the changing nature of manufacturing and growth in the service economy. The proposal reflects a change from previous industrial use to commercial use and will address the issue identified of a lack of local employment (with most residents employed outside of the municipality).

The subject site is located within the Brunswick Major Activity Centre, a higher order centre where a major Trade supplies and Restricted retail premises business should be located. The proposed building will efficiently utilise its location within an inner-suburban context with the proposed built form in keeping with the contemporary mid-rise built form character of its surrounds, integrating with the existing streetscape.

The proposal provides for increased employment opportunities within the activity centre. The proposed development provides for a significant increase in employment of the site, in the order of 90 jobs from the existing use.

The design response has given careful consideration to the need to mitigate potential amenity and environmental impacts on nearby residential properties. A light court is located on the eastern façade against an adjoining light court for the residential property at 191-193 Lygon Street.

The proposed building faces Glenlyon Road, with extensive glazing across the ground and first floor façades. The presentation of the building to Glenlyon Road will assist in revitalising and activating the public realm. The proposal has comprehensively considered and responded to its varied interfaces which are well documented via the interface study found in Appendix B. The ground floor will establish a positive relationship with the street through an active frontage, which will provide passive surveillance and enhance the public realm.

The Sustainability Management Plan prepared by Sustainable Development Consultants demonstrates that the proposed development incorporates environmentally sustainable design to ensure ongoing energy efficiency and excellent stormwater management.

The proposal will contribute to local economic activity in keeping with its physical and strategic context. The proposed Trade supplies and Restricted retail premises use will encourage the growth of Brunswick Major Activity Centre and provide increased local employment opportunities to support the local economy.

5.1.2 Clause 22 – Local Planning Policies

Clause 22.03 sets out Council's policy for applications that are to be assessed against the requirements of Clause 52.06 (Car Parking). It is policy to support reduced car parking rates for uses which are within close proximity to activity centres and which have excellent access to a range of public transport options. An increased provision of bicycle parking above the rates specified in Clause 52.34 is encouraged.

Clause 22.03 supports Council's commitment to transport systems that encourage sustainable communities. It is policy to:

- *Support reduced car parking rates in developments within and in close proximity to activity centres, with excellent access to a range of public transport options and with increased provision of bicycle parking above the rates specified in clause 52.34.*
- *Ensure development provides adequate on-site loading areas for freight, commercial and waste collection vehicles where appropriate.*
- *Limit the number of vehicle crossings to one per site frontage, other than on corner lots.*
- *Ensure vehicle crossing provision limits the removal of on street public parking spaces, removal of street trees, and encroachment into landscaped front setbacks, and maximises pedestrian safety and sight lines.*

Response

Trade supplies and Restricted retail premises such as Bunnings require considerable on-site car parking given that the goods being purchased are often large and heavy and not practical to transport via active modes of travel or on public transport. Therefore, a reduction in on-site car parking below a nominated statutory rate, as may often be appropriate for a site within a higher order activity centre, is not practical or consistent with orderly planning having regard to the proposed use.

The proposal makes use of the one existing vehicle crossover located at the south-west corner of the site onto Glenlyon Road. A carriageway easement is to be established across the eastern end of 6 Pitt Street to provide sufficient space for an accessway for the movement of service and trade vehicles along the access leg of the subject site onto Pitt Street (exit only). A new entry crossover is to be located for service vehicles at the south-eastern corner of the site onto Glenlyon Road. The separation of heavy vehicles associated with deliveries and retail customers minimises the potential for conflict between such vehicles.

The large size of the site and the proposed use justifies the establishment of a second crossover onto Glenlyon Road, noting the generous separation between the two crossovers. The proposal does not require the removal of any street trees. Sufficient sight lines are also proposed in order to maximise pedestrian safety.

Clause 22.04 sets out Council's policies for advertising signages, relevant policies under this clause including the following:

- *Ensure that signs respect the style and scale of the host building, nearby buildings and the character of the street.*
- *Ensure that signs do not cause visual clutter.*
- *Ensure that signs fit within architectural forms and are integrated with the design of the building.*
- *Ensure that signs do not obscure architectural features of buildings, including windows.*
- *Ensure that existing and proposed signage is rationalised.*
- *Ensure that signs, including supporting structures, are designed with consideration for views from all angles.*
- *Ensure that wall or fascia signs are applied directly to the building or on a flush mounted panel with minimum projection.*
- *Ensure that signs are modest in scale and are sensitive to the residential character and amenity of the area.*
- *Ensure that signs do not block out ground level display windows.*
- *Ensure that signage is consistent with any established pattern of signage.*

Response

The proposed business identification signage is to be located on the southern, northern and western façades of the building (above the ground floor glazing on the southern facade). The signage will be integrated with the design of the building and will not obscure any architectural features or windows. The proposed signage is in keeping with a commercial / industrial area within a higher order activity centre. The signage will not impact upon views or vistas.

There will be no impact on road safety resulting from the signage, which is of sufficient clarity, with limited text and graphics, to ensure that drivers are able to view it without distraction. The nearest controlled intersection (Lygon Street & Glenlyon Road) is approximately 85 metres from the site, which is a sufficient distance to ensure that the signage will not cause distraction at a key decision-making point.

All proposed externally illuminated signage (to the northern and the southern part of the western facade) will be switched off from 11pm and has been appropriately set back from sensitive residential interfaces to ensure the ongoing amenity of these residents.

Clause 22.08 recognises the importance of environmental sustainability. This Clause addresses a number of issues including water quality and energy usage with the objective that development should achieve best practice in environmentally sustainable development from the design stage through to construction and operation.

Response

Integrated passive and active sustainable design will aid in the delivery of an energy efficient, water efficient and healthy building.

In terms of performance outcomes, the analysis presented in the Sustainability Management Plan, prepared by Sustainable Development Consultants, demonstrates that the proposed development meets the standard of commercial building envelope energy efficiency required to satisfy the Building Code of Australia. Furthermore, the combination of design features and services initiatives meets the standards of the BESS rating tool.

The Sustainability Management Plan demonstrates that the proposal will achieve best practice in regard to environmentally sustainable development.

6.0 Planning Controls

6.1 Permit Triggers

We have identified the following planning permit triggers for this application:

- Clause 33.03-1 (Industrial 3 Zone) – for use of land for Trade supplies and Restricted retail premises (Section 2 Uses) and buildings and works associated with a Section 2 Use
- Clause 43.02-2 (Design and Development Overlay) – Schedule 19 - to construct a building or carry out works
- Clause 45.06-3 (Development Contributions Plan Overlay) – Schedule 1 – Development Infrastructure Levy
- Clause 52.02 Easements, Restrictions and Reserves – to create an easement.
- Clause 52.05 Signs – for the display of floodlit business identification signage with a total display area of over 8 square metres
- Clause 52.34 Bicycle Facilities – to reduce the statutory bicycle parking requirement

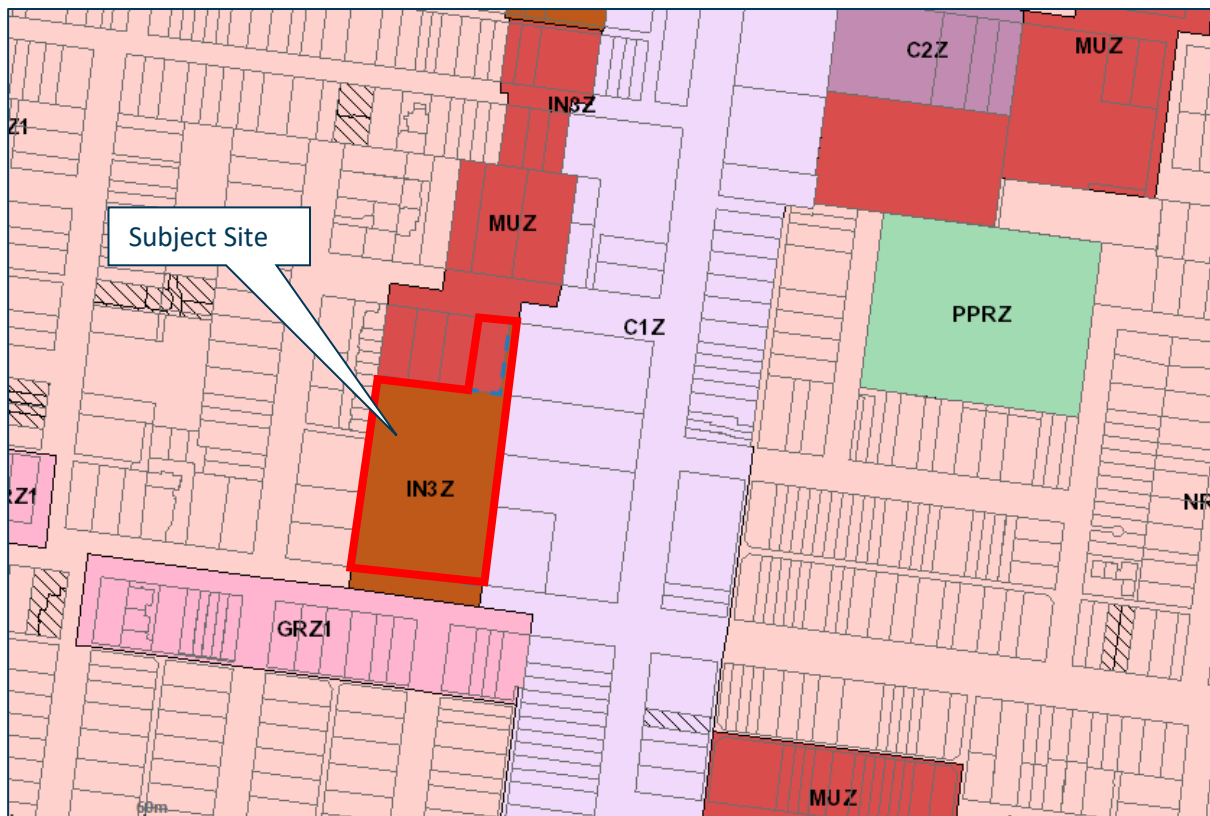
6.2 Zoning

The subject site is located predominantly within the Industrial 3 Zone with 6 Pitt Street within the Mixed Use Zone. The surrounding land is zoned in a mix of zones as follows:

- Neighbourhood Residential Zone to the west and north-west
- Mixed Use Zone to the north
- Commercial 1 Zone to the east
- General Residential Zone to the south

The land in the wider surrounding area is predominantly zoned Neighbourhood Residential Zone. Refer Figure 5.

Figure 5: Moreland Planning Scheme – Zoning Map



Source: VicPlan

The purpose of the Industrial 3 Zone includes:

- *To allow limited retail opportunities including convenience shops, small scale supermarkets and associated shops in appropriate locations.*
- *To ensure that uses do not affect the safety and amenity of adjacent, more sensitive land uses.*

The use of land for Trade supplies and Restricted retail premises requires a planning permit within the Industrial 3 Zone. Buildings and Works associated with these proposed uses also requires a planning permit.

Clause 33.03-2 requires that a new use must not adversely affect the amenity of the neighbourhood, including through the transport of materials and goods to or from the land, appearance of any stored goods and emission of noise, artificial light, odour, waste water or water products.

Relevant decision guidelines include:

- *The effect that the use may have on nearby existing or proposed residential areas or other uses which are sensitive to industrial off-site effects, having regard to any comments or directions of the referral authorities.*
- *The availability of and connection to services.*
- *The effect of traffic to be generated on roads.*

Clause 33.03-4 set out the permit requirements for buildings and works applications within the Industrial 3 Zone, a permit is required to construct a building or carry out works.

Relevant decision guidelines include:

- *Streetscape character.*
- *Built form.*
- *Interface with non-industrial areas.*
- *The availability of and connection to services.*
- *The effect of traffic to be generated on roads.*

Response

We submit that the applicable zone for this application is the Industrial 3 Zone given that the subject site occupies only a small portion of land within the Mixed Use Zone and only an easement is proposed over the Mixed Use Zone portion of the site.

The proposed Trade supplies and Restricted retail premises uses will make more efficient use of land that is fully serviced and which has access to a wide range of infrastructure and services. The site offers good access to road links and public transportation and will contribute positively to local economic growth within a higher order activity centre.

The proposed use will not impact the amenity of surrounding properties. Potential noise impacts have been thoroughly considered in the acoustic assessment prepared by Octave Acoustics.

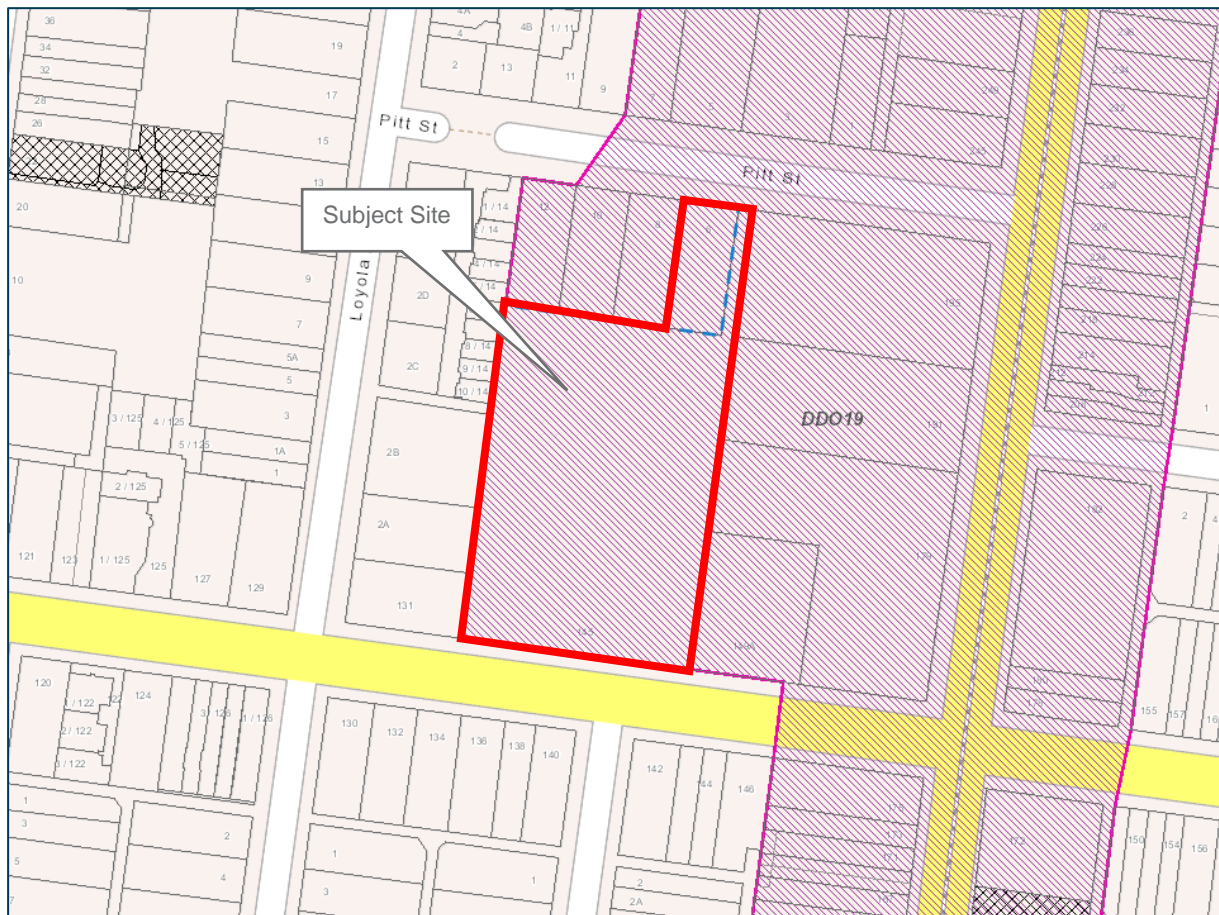
The Traffic Assessment Report prepared by TTM Consulting demonstrates that the proposed use will not impact upon the safe and efficient use of the existing local road network.

The State and local planning policies of the Moreland Planning Scheme provide considerable support for the redevelopment of the subject site for the purposes and intensity proposed. The design response achieves an appropriate balance between the efficient use of land with the protection of the amenity of adjoining properties.

6.3 Overlays

The Design and Development Overlay (Schedule 19) – titled *Brunswick Activity Centre – Lygon Street Local Area*, has been applied to land within the Brunswick Major Activity Centre. Refer Figure 6.

Figure 6: Moreland Planning Scheme – Design and Development Overlay



Source: Planning Maps Online

The design objectives of Schedule 19 to the Design and Development Overlay include:

- *To create a new mid rise built form character that accommodates buildings generally ranging from 3 to 9 storeys.*
- *To ensure building design incorporates best practice environmentally sustainable design initiatives.*
- *To create an inviting, safe and vibrant public realm.*
- *To maintain reasonable amenity for residential properties adjacent to and within the activity centre.*

The following built form parameters are nominated for the subject site:

- Preferred (discretionary) maximum building height of 14 metres
- For land with a setback to residential zoned land outside of the activity centre (relevant to the western boundary of the subject site) a new building should be setback from the residential boundary by a dimension equivalent to its height above 5 metres, up to a maximum setback of 10 metres (so a maximum height of 5 metres on the boundary).

The street wall requirements do not apply to the subject site. The schedule to the overlay sets out active edge requirements for street frontages (via reference to the Brunswick Structure Plan), however they do not apply to the site.

New development should incorporate windows in all upper level facades facing streets.

The impact of vehicle access and car parking on the public realm is to be minimised by locating vehicle access to the rear where possible or on secondary frontages and by locating car parking where it cannot be seen from the public realm. Services cabinets are to be located to the rear of buildings where possible. Where they can only be located in the front façade, the size of services cabinets is to be minimised and they should be integrated with the overall façade design.

Relevant decision guidelines set out in the Schedule to the overlay include:

- *The Brunswick Structure Plan (Local Area Two – Lygon Street)*
- *The opportunities and constraints of the site.*

Response

The proposed development is consistent with the design objectives of the schedule to the overlay. The proposal is for a mid-rise building which has a maximum height of 15.4 metres (equivalent to four residential storeys) which is appropriate for a large site such as this.

The street wall height along Glenlyon Road is 14.2 metres in height and increases gradually to a parapet height of 15.4 metres, set back 7.3 metres from the southern site boundary.

The architectural plans include the residential setback guideline on the sectional drawings for the interface with the residential zoned land to the west and demonstrate that the proposed building is largely in compliance with this standard, with only very minimal non-compliances. This demonstrates that a very reasonable approach has been taken with respect to building height and massing to the residential zoned interface of the site (to the west).

The façade fronting Glenlyon Road provides for passive surveillance and enhances the public realm through extensive glazing.

The design response achieves efficient utilisation of the land in keeping with the outcomes sought for land in the Brunswick Major Activity Centre.

The Development Contributions Plan Overlay (Schedule 1) applies to the subject site. Refer Figure 7.

Figure 7: Moreland Planning Scheme – Development Contributions Plan Overlay



Source: [Planning Maps Online](#)

A Development Infrastructure Levy applies to the proposal under the Development Contributions Plan Overlay.

Response

We understand a development Contributions levy will apply to the proposal as part of the issue of the planning permit.

6.4 Particular Provisions

6.4.1 Clause 52.02 – Easements, Restrictions and Reserves

The purpose of Clause 52.02 is to enable the removal and variation of an easement or restriction to enable a use or development in compliance with the planning scheme.

A permit is required to create vary or remove any easement.

Before deciding on an application, the responsible authority must consider the interests of all affected people.

Response

Heavy vehicles (for deliveries and waste collection) and trade customers will exit via an accessway which is located partially across land known as 6 Pitt Street, to the north of the subject site.

A 2.84-metre-wide carriageway easement is proposed to the eastern boundary of 6 Pitt Street.

This easement will allow for a 6.5-metre-wide carriageway width as required for heavy vehicles exiting the proposed development onto Pitt Street.

A Plan for the Creation of an Easement, prepared by Veris accompanies this planning permit application.

6.4.2 Clause 52.05 – Signs

The purpose of Clause 52.05 is to regulate the display of signs and to provide signs that are compatible with the amenity and visual appearance of an area, including the existing of desired future character. Signs are not to contribute to excessive visual clutter or visual disorder, while ensuring that the signs do not cause a loss of amenity or adversely affect the nature or built environment or the safety, appearance or efficiency of a road.

A permit is required for floodlit business identification signage exceeding a total display area of eight square metres in industrial areas. The following decision guidelines (applicable to all types of advertising signage) are set out in Clause 52.05 and are of relevance to this application:

- *The character of the area*
 - *Impacts on views and vistas*
 - *The relationship to the streetscape, setting or landscape*
 - *The relationship to the site and building*
 - *The need for identification and the opportunities for adequate identification on the site or locality*
 - *The impact on road safety*
-

Response

The proposed business identification signage is located on the southern, northern and western façades of the proposed development.

The proposed signage is in keeping with the context of the site within a higher order activity centre. The signage will not impact upon views or vistas.

All proposed externally illuminated signage (to the northern and the southern section of the western facade) will be switched off at 11pm and has been appropriately set back from sensitive residential interfaces to ensure the ongoing amenity of these residents.

There will be no impact on road safety resulting from the signage, which is of sufficient clarity, with limited text and graphics, to ensure that drivers are able to view it without distraction. The nearest controlled intersection is approximately 85 metres from the site, which is sufficient distance to ensure that the signage does not distract drivers at this intersection.

6.4.3 Clause 52.06 - Car Parking

The purpose of Clause 52.06 is to ensure that sufficient and appropriately designed car parking facilities are provided for new developments commensurate with the demand generated by the intended use of the site, having regard to the nature of the locality.

Clause 52.06 also aims to ensure that car parking is appropriately designed and located so that it does not impact upon the amenity of the locality, including existing residents or pedestrians and other road users and achieves a high standard of urban design. Car parking areas should be designed so that they are safe and easy and efficient to use.

A new use must not commence or the floor area of an existing use must not be increased until the required car parking has been provided on the land. A permit may be granted to reduce or waive the number of car spaces required. The Clause specifies the rate at which car parking is to be provided according to the proposed use of the site.

A summary of the requirements of this clause in so far as they relate to this application is provided in the following table (noting that the applicable planning scheme rate is from Column B due to the site’s location within the Principal Public Transport Network Area (PPTNA)). Refer to Table 2.

Table 2: Car parking rates specified in Clause 52.06			
Use	Planning Scheme Rate	Car spaces required	Total car spaces provided
Trade Supplies	10% of site area (5,395 sqm – 75% of total)	22	250
Restricted retail premises	2.5 spaces to each 100 sqm of leasable floor area (2,174 sqm – 25% of total)	54	

While not practical for most customers, staff will have excellent access to local tram and bus services. Tram routes 1 and 6 running along Lygon Street and Bus route 506 operates along Glenlyon Road and provide convenient access to Melbourne CBD and surrounding areas.

Clause 52.06 also requires the preparation of a plan showing the location and dimensions of car parking spaces, access lanes, driveways and associated works and landscaping. These details have been provided in the plans by Stokes Architects.

Response

The plans demonstrate that the on-site car parking satisfies the statutory car parking requirement.

A comprehensive Traffic Impact Assessment (TIA) prepared by TTM Consulting, forms part of our planning permit application submission. The TIA finds that the additional traffic generated is not anticipated to have any adverse impacts on the safety and operation of the surrounding road network.

The proposed on-site car parking provision is appropriate notwithstanding the location of the site within a higher order activity centre, given the impracticality of alternative modes of travel having regards to the intended use, where the goods typically purchased are large and bulky and not easily able to be transported by hand. The statutory car parking rates are well below those experienced at Bunnings, particularly at peak times, so the surplus number of spaces above the statutory rates is considered appropriate and will cater for all demand generated by the use.

The architectural plans demonstrate that the proposed car parking layout has been designed to be safe and convenient for pedestrians and visitors and will allow vehicles to enter and exit the site in a forwards direction.

6.4.4 Clause 52.34 – Bicycle Facilities

The purpose of Clause 52.34 is to encourage cycling as a mode of transport. Secure, accessible and convenient bicycle parking spaces are to be provided for higher density developments with associated shower and change facilities where Trade supplies and Restricted retail premises uses are proposed.

The bicycle requirements for the proposed Trade supplies and Restricted retail premises are as follows:

Table 3: Bicycle Parking Requirement in Clause 52.34				
Use	Proposal	Planning Scheme Rate	Total bicycle spaces required	Total bicycle spaces provided
Trade supplies and Restricted retail premises	8495m ² available to the public	Staff – 1 to each 300m ² of leasable floor area	28 spaces	14 spaces
		Customer – 1 to each 500m ² of leasable floor area	17 spaces	
TOTAL			45 spaces	14 spaces

Response

The proposed number of bicycle spaces is less than the number specified, however the provision of bicycle parking is considered practical given the proposed use and the reduced likelihood of customers travelling to the

site by bicycle given the large and bulky goods typically purchased. The bicycle spaces for customers and staff are provided for on the footpath in front of the subject site and within the basement car park.

6.4.5 Clause 53.18 – Stormwater Management in Urban Development

The purpose of Clause 53.18 is to ensure that stormwater is carefully treated in urban development, including retention and reuse, and is managed to mitigate the impacts of stormwater on the environment, property and public safety.

To encourage development that reduces the impact of stormwater on the drainage system and filters sediment and waste from stormwater prior to discharge from the site.

The following decision guidelines are set out in Clause 53.18 and are of relevance to this application:

- *The capacity of the site to incorporate stormwater retention and reuse and other water sensitive urban design features*
 - *Whether the development has utilised alternative water sources and/or incorporated water sensitive urban design*
 - *Whether stormwater discharge from the site will adversely affect water quality entering the drainage system*
 - *The capacity of the drainage network to accommodate additional stormwater*
 - *Whether the stormwater treatment areas can be effectively maintained*
 - *Whether the owner has entered into an agreement to contribute to off-site stormwater management in lieu of providing an on-site stormwater management system*
-

Response

The proposal will incorporate appropriate implementation and management to reduce the impact of stormwater runoff on the urban drainage system.

Rainwater will be collected from all roof surfaces and stored in rainwater tanks with a minimum effective storage capacity of 100,000L. The water will be used for the flushing of all toilets across the development, with excess water to be used for watering of plants in the outdoor nursery.

The Sustainable Management Plan prepared by Sustainable Development Consultants demonstrates that the proposed development will treat the stormwater generated on-site efficiently and achieve best practice in regard to environmentally sustainable development.

7.0 Amendment C193

Amendment C193 to the Moreland Planning Scheme proposed to rezone industrial land in the Brunswick Structure Plan Area to implement the Brunswick Structure Plan and MILS. The Amendment will implement the objectives of Clause 21.03. The amendment aims to support the transition from traditional industrial uses to a broader range of employment uses and prioritises employment uses over residential uses.

Amendment C193 proposed to rezone 55 properties including the subject site to Commercial 3 Zone with a maximum 50% residential floor area requirement (minimum 50% commercial floor area).

Response

The proposal is consistent with the intent of Amendment C193 and the categorisation of the subject site within an Employment Area (Category 2) as identified in the Brunswick Structure Plan.

The proposed Trade supplies and Restricted retail premises use will provide for a significant increase in employment on the subject site in the order of 90 jobs when compared to existing conditions, consistent with the designation of the site as within an Employment Area in the Brunswick Structure Plan and MILS.

The proposal is consistent with the outcomes sought for this site as set out in the relevant provisions of the Moreland Planning Scheme. The proposal will support and facilitate economic activity in the local area.

8.0 Conclusion

The proposal is consistent with the aspirations of Council for land within the Brunswick Major Activity Centre as detailed in this report.

The proposed Trade supplies and Restricted retail premises use represents an efficient use of land in a well-serviced area within a higher order activity centre on a site that is currently under-utilised. The proposal will support and strengthen economic activity and will result in a significant increase in employment opportunities within the Brunswick Major Activity Centre within an additional 90 jobs created when compared to the existing land use.

The Traffic Impact Assessment demonstrates that the proposed access and on-site car parking arrangements are safe, efficient and appropriate having regard to the location and the proposed use. Access for heavy vehicles (for deliveries and waste collection) have been appropriately designed for, as has access for trade customers which are separated from general customers.

Due regard has been given to the sensitive residential interfaces of the site with all potential amenity impacts appropriately responded to, including visual bulk, overshadowing and noise.

The proposal is consistent with the outcomes sought for this site as set out in the relevant provisions of the Moreland Planning Scheme and the Brunswick Structure Plan which together set out the strategic context of the site.

For the reasons outlined in this report it is considered that the design response is appropriate to both the broader site context and its immediate context and is therefore worthy of planning permission.

Appendix A – Site Photographs

Photo 1: Looking north at part of the frontage of the subject site (145 Glenlyon Road) from the southern side of Glenlyon Road.



Photo 2: Looking north at part of the frontage of the subject site and adjoining property (145 and 149 Glenlyon Road) from the southern side of Glenlyon Road.



Photo 3: Looking north along the western common boundary from the southern side of Glenlyon Road with the subject sit on the right and 133 Glenlyon Road on the left.



Photo 4: Looking west from the subject site at the shared common western boundary with 2A Loyola Avenue.



Photo 5: Looking south from the northern side of Pitt Street at the proposed access way of the subject site.



Photo 6: Looking north west from the subject site at the shared common boundary between 14 Pitt Street and the subject site



Photo 7: Looking south east from Pitt Street at the western elevation of 195 – 197 Lygon Street facing the common boundary to the subject site



Photo 8: Looking west from the northern side of Glenlyon Road directly in front of the subject site.



Photo 9: Looking east from the northern side of Glenlyon Road directly in front of the subject site (from the vehicle accessway).



Photo 10: Looking south from 179 Lygon Street (service station) at the shared common boundary with 145 and 149 Glenlyon Road



Appendix B – Property Interfaces Review

145 Glenlyon Road, Brunswick



Source: Nearmap – Date: 17 December 2019

133 Glenlyon Road, Brunswick



Source: Nearmap – Date: 17 December 2019

- Land use – multi dwelling development (late 20th century)
- Building setback from common boundary = approximately 7 m
- Solar access implications – limited – site is to the west of subject site
- Use of immediately adjacent land – driveway/ hardstand to communal car parking area
- Existing conditions - open accessway along common boundary interface



East elevation of 133 Glenlyon Road and 2A Loyola Avenue

The figure above illustrates the existing conditions of the western boundary of the subject site. A hardstand communal car parking area is located along the common boundary.



East elevation of 133 Glenlyon Road and 2A Loyola Avenue, showing the glazing facing the subject site

The figure above illustrates the glazing on the eastern elevation of 133 Glenlyon Road and 2A Loyola Avenue, each building features six windows that are facing the subject site. the distance between the buildings and the proposed building is approximately 10 metres.

2A Loyola Avenue, Brunswick



Source: Nearmap – Date: 31 August 2019

- Land use – multi dwelling development (late 20th century)
- Building setback from common boundary = approximately 7m
- Solar access implications – limited – site is to the west of subject site
- Use of immediately adjacent land – driveway/ hardstand to communal car parking area
- Existing conditions - open accessway along common boundary interface (refer Figure 1 and 2 for east elevation and glazing)



Looking west at the shared common boundary with 2A Loyola Avenue

There is evergreen screening vegetation located on the common boundary, which screens views between the properties.

2B Loyola Avenue, Brunswick



Source: Nearmap – Date: 17 December 2019

- Land use – multi dwelling development (late 20th century)
- Building setback from common boundary = approximately 6m
- Solar access implications – limited – site is to the west of subject site
- Use of immediately adjacent land – hardstand to communal car parking area
- Existing conditions - open accessway along common boundary interface



Looking west at the shared common boundary with 2B Loyola Avenue

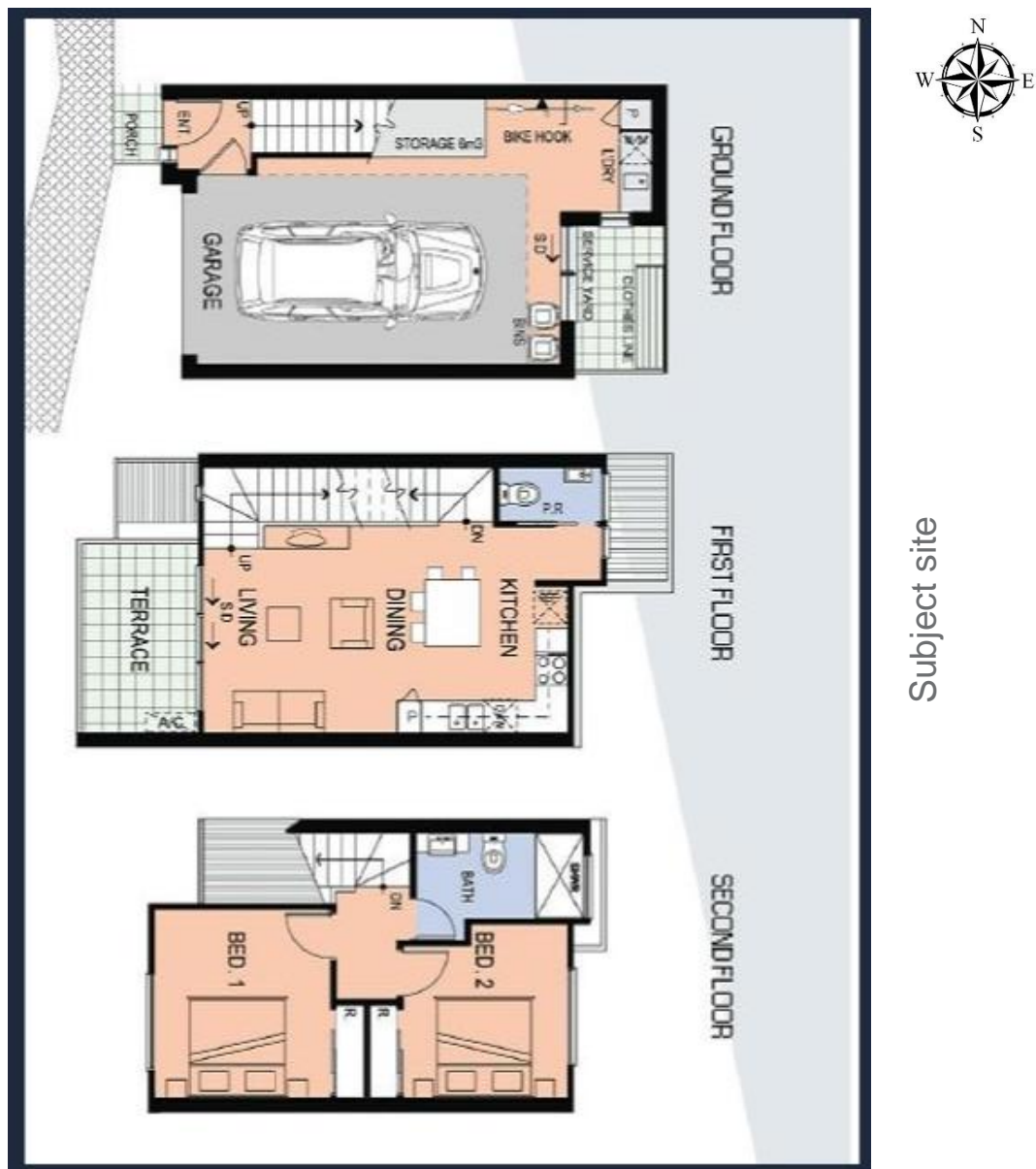
14 Pitt Street, Brunswick

(6 – 10/ 14 Pitt Street share a common boundary with the subject site)



Source: Nearmap – Date: 17 December 2019

- Land use – multi dwelling development (constructed 2014)
- Building setback from common boundary = approximately 1m (parts of the development appear to be built to the boundary)
- Solar access implications – located to immediate west with limited setback
- Use of immediately adjacent land – service type courtyards (this is not the primary secluded private open space of the dwellings – which is located on elevated balconies facing west)
- Existing conditions – wall of the shed constructed on common boundary is approximately 3 metres in height (refer figure below)





Looking north west from the subject site at the shared common boundary between 14 Pitt Street and the subject site

The image above illustrates east facing glazing of 14 Pitt Street, habitable room windows can be observed on second floor (refer floor plan). The multi-dwelling development is setback from the common boundary by approximately 1 metre.



Looking north at 14 Pitt Street from its south western corner

The image above shows the west elevation of 14 Pitt Street, which includes first floor balconies as primary secluded private open spaces.

12 Pitt Street, Brunswick



Source: Nearmap – Date: 17 December 2019

- Land use – double storey single dwelling (late 20th century)
- Building setback from common boundary = Nil – ground floor of the dwelling is built on the common boundary with first floor setback approximately 8 m and terrace on roof space above
- Solar access implications – nil – site is to the north of subject site
- Use of immediately adjacent land – residential use/ terrace
- Existing conditions – wall on boundary approximately 3 metres high

10 Pitt Street, Brunswick



Source: Nearmap – Date: 17 December 2019

- Land use – single storey dwelling (late 20th century)
- Building setback from common boundary = approximately 8m (an outbuilding is constructed to the common boundary in the south east corner of this site)
- Solar access implications – nil – site is to the north of subject site
- Use of immediately adjacent land – Secluded private open space / outbuilding
- Existing conditions – approximately 3m high wall constructed to the boundary

6 - 8 Pitt Street, Brunswick



Source: Nearmap – Date: 17 December 2019

- Land use – commercial kitchen in demountable buildings
- Building setback from common boundary = 0m
- Solar access implications – nil – site is to the north of subject site
- Use of immediately adjacent land – commercial kitchen
- Existing conditions – wall on boundary has a height of approximately 3 metres



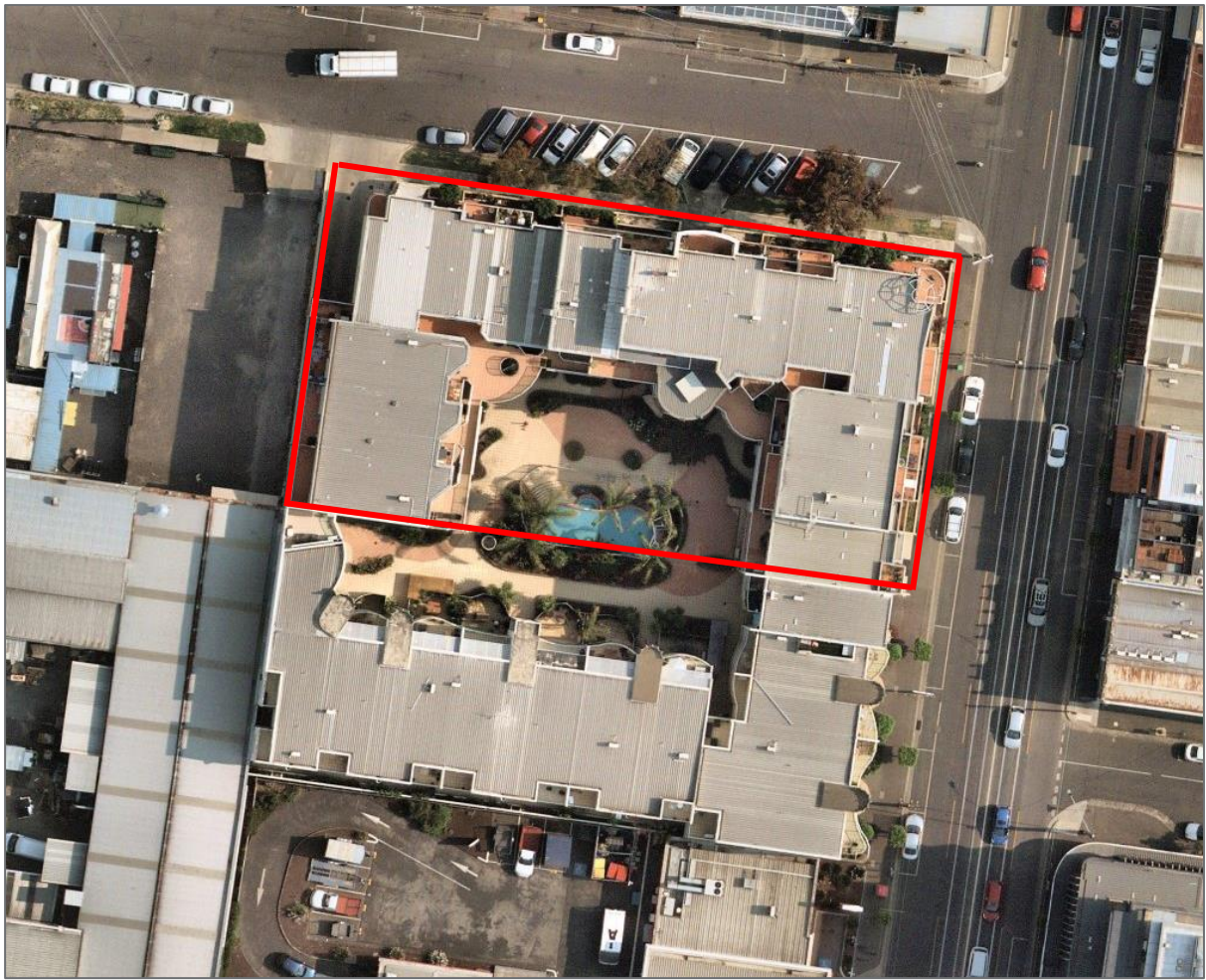
Looking south from the entrance of 6 – 8 Pitt Street at KitchenCo



Looking south from the north side of Pitt Street

The image illustrates existing conditions at the interface of the proposed accessway to the subject site from Pitt Street.

195-197 Lygon Street, Brunswick East

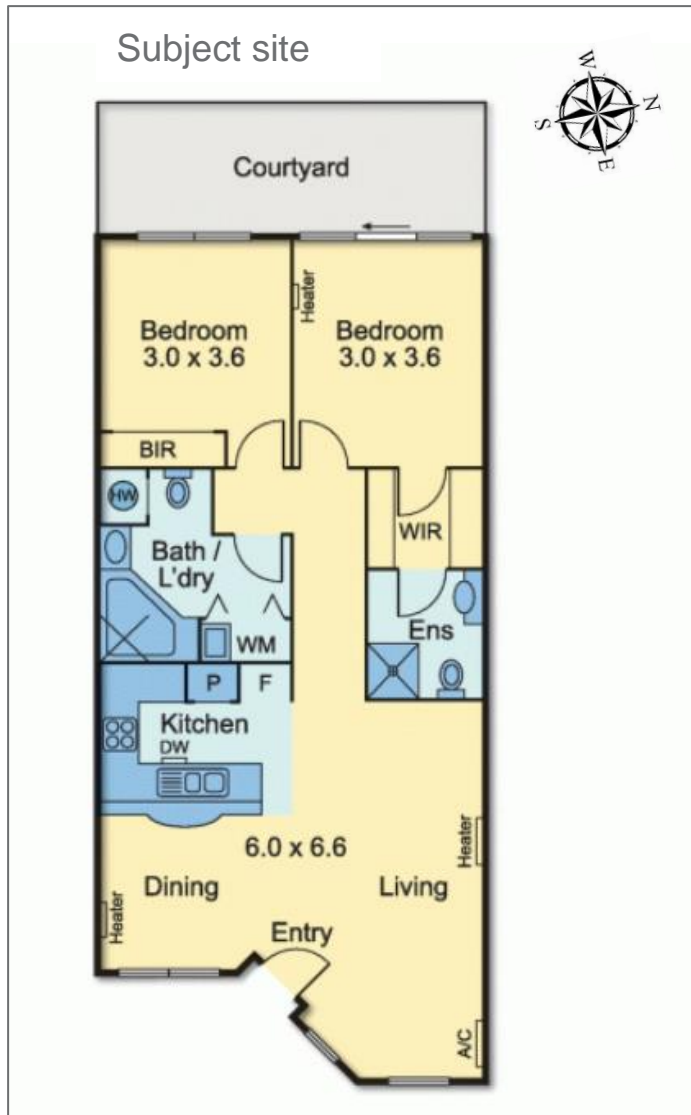


Source: Nearmap – Date: 17 December 2019

- Land use – multi-dwelling development (late 1990s)
- Building setback from common boundary = nil
- Solar access implications – site is to the east of subject site
- Use of immediately adjacent land – courtyard/ terrace (residential use)
- Existing condition – terraces and courtyards near the common boundary abutting accessway

Floor Plan – 2/ 195-197 Lygon Street

(Ground floor apartment on common boundary with subject site)



Bedrooms are adjacent to the courtyard abutting the proposed accessway between the subject site and Pitt Street on the ground floor level. On levels above this layout appears to have been repeated. All apartments appear to be inwards (east) facing in terms of the living areas.



Looking south east from Pitt Street at the western elevation of 195 - 197 Lygon Street facing the common boundary to the subject site

The image above illustrates the western elevation of 195 - 197 Lygon Street, courtyards and balconies can be observed and are adjacent to the existing accessway between the subject site and Pitt Street.



Looking at the north eastern corner of the subject site

191 – 193 Lygon Street, Brunswick East

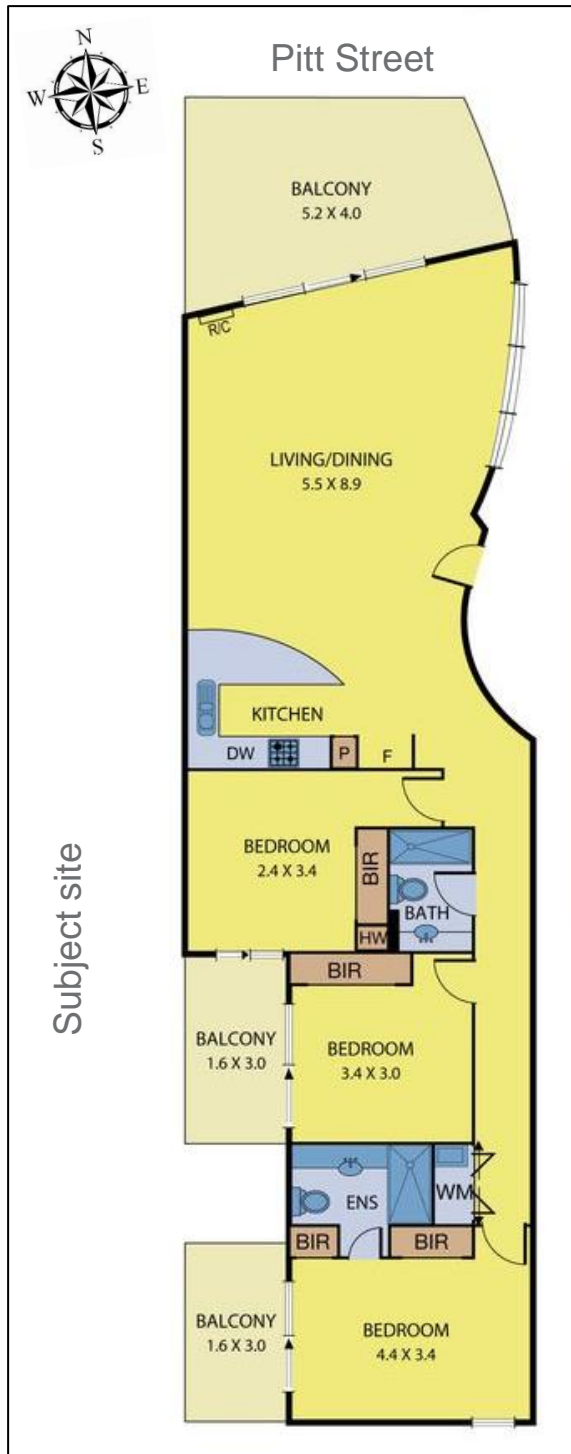


Source: Nearmap – Date: 17 December 2019

- Land use – multi-dwelling development (late 1990s)
- Building setback from common boundary = nil
- Solar access implications – site is to the east of Subject site
- Use of immediately adjacent land – residential use
- Existing condition – wall constructed on common boundaries of both lots

Floor Plan – 35/ 191-193 Lygon Street

(third storey apartment on common boundary with subject site)



On the level immediately below (second storey and the level above (top storey)) this layout appears to have been repeated. All apartments appear to be inwards (east) facing in terms of the living areas, while the balconies appear to have outlook to the subject site through a cut out in the boundary wall.



Looking north at the common boundary between the subject site and 191 – 193 Lygon Street from the carwash at 179 Lygon Street

The existing (eastern) boundary wall of the subject site is equivalent to two storeys in height, with a height of approximately 6 metres. The apartment building 191-193 Lygon Street is built on the common boundary with the subject site.



Looking east from the existing on-ground parking area at the subject site at 191-193 Lygon Street

The figure illustrates western glazing and light well of 191-193 Lygon Street with two-bedroom room windows on each floor facing towards the subject site.

179 Lygon Street, Brunswick East



Source: Nearmap – Date: 17 December 2019

- Land use – Carwash and service station / convenience store
- Building setback from common boundary = substantial
- Solar access implications – Nil – not a sensitive use
- Use of immediately adjacent land – accessway for carwash
- Existing condition – the wall on common boundary is approximately 6 metres in height (refer Figure 11)

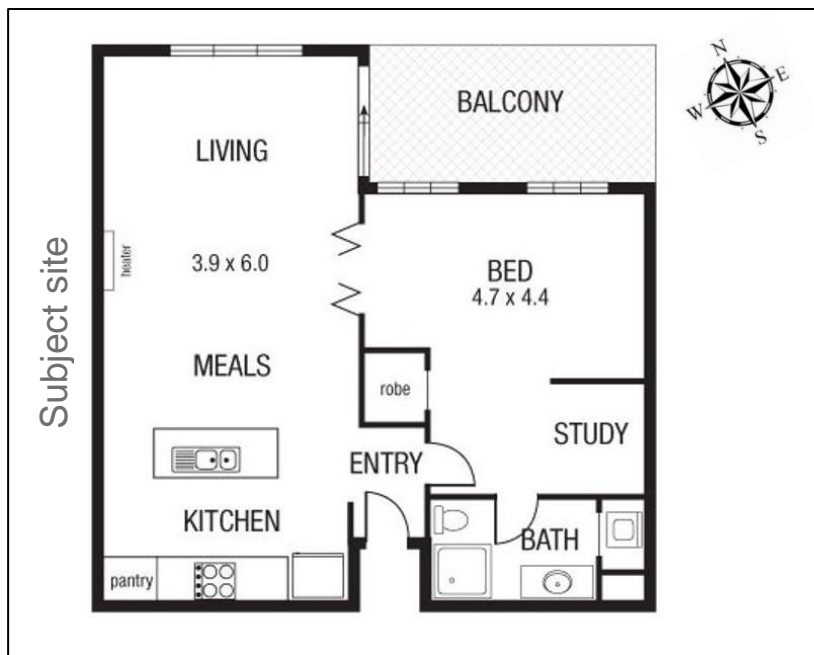
149 Glenlyon Road, Brunswick



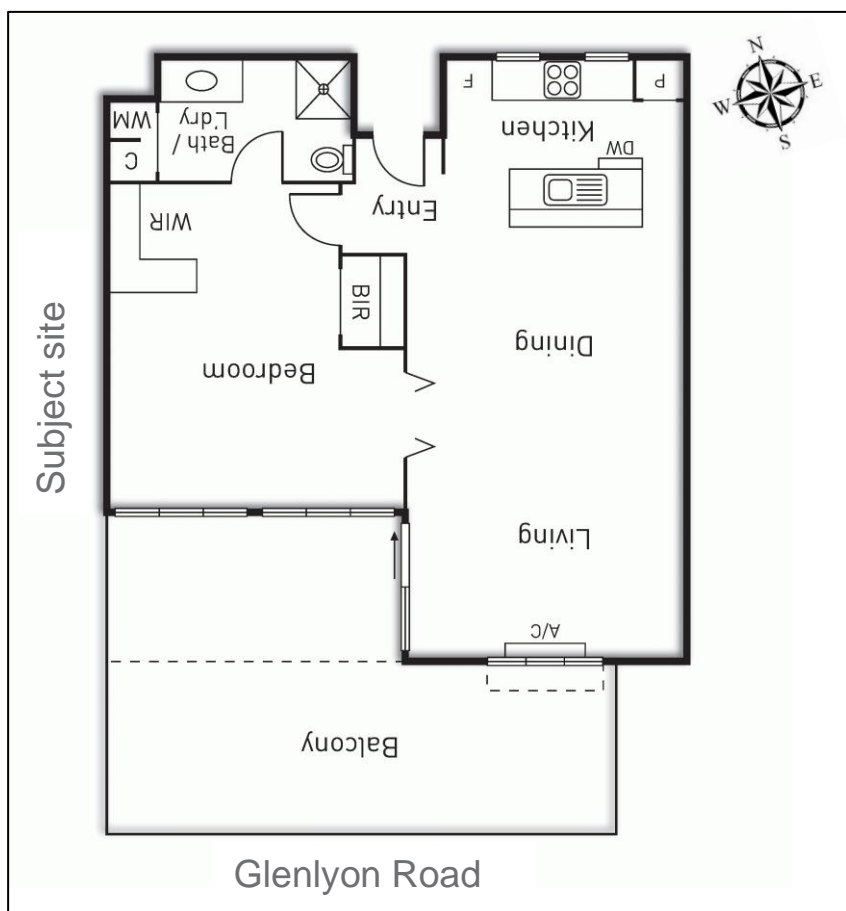
Source: Nearmap – Date: 17 December 2019

- Land use – multi-dwelling development (late 1990s)
- Building setback from common boundary = nil
- Solar access implications – site is to the east of subject site
- Use of immediately adjacent land – residential
- Existing condition – the wall constructed on the common boundary is approximately 6 metres in height and the wall constructed at 149 Glenlyon Road is approximately 12 metres in height

Floor Plan – 15/ 149 Glenlyon Road – North facing apartment



Floor Plan – 8/ 149 Glenlyon Road – south facing apartment





Looking south from 179 Lygon Street (service station) at the shared common boundary with 145 and 149 Glenlyon Road

A projecting boundary wall with a height of approximately 12 metres is constructed at 149 Glenlyon Road along the common boundary with 145 Glenlyon Road.